

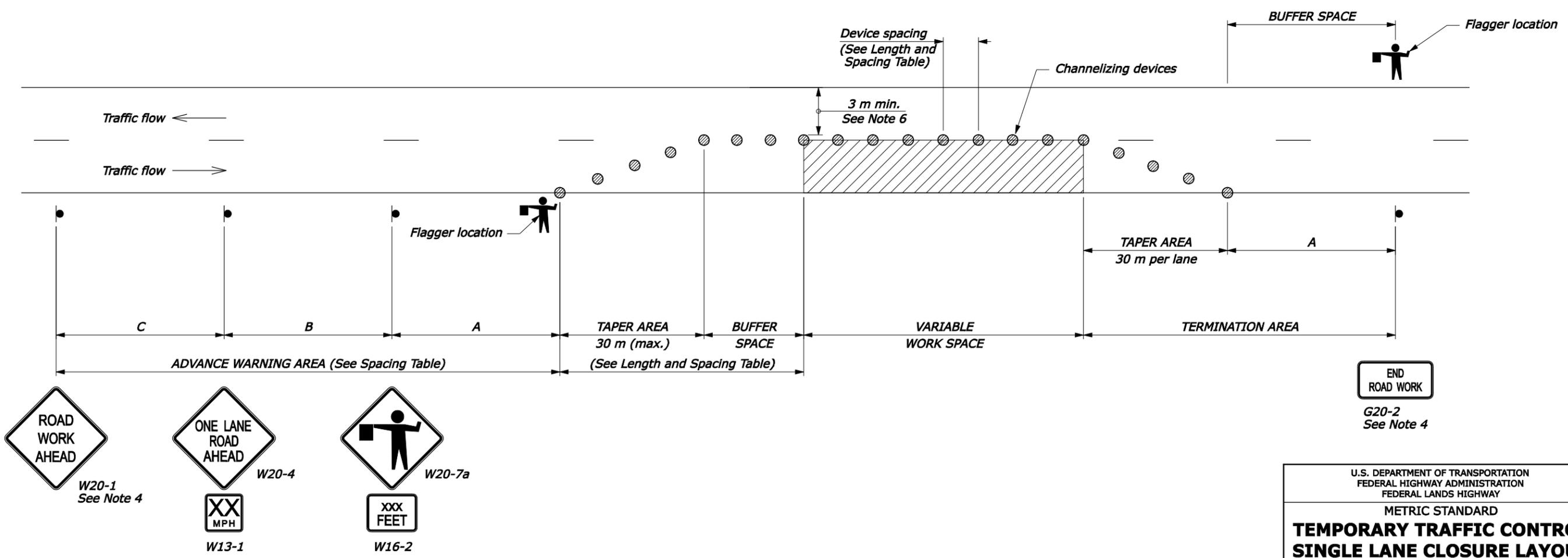
LENGTH AND SPACING TABLE					
APPROACH SPEED*		LENGTH OF BUFFER SPACE METER	CHANNELIZING DEVICE		
MPH	km/h		TAPER AREA	BUFFER SPACE	WORK SPACE
25	40	50	6	15	15
30	50	65	6	18	18
35	55	75	6	21	21
40	65	95	6	24	24
45	70	105	6	27	27
50	80	130	6	30	30
55	90	160	6	33	33

* Approach speed based on the regulatory posted speed, not the advisory speed.

ROAD TYPE	DISTANCE BETWEEN SIGNS IN METERS		
	A	B	C
Urban less than 70 km/h [≤ 40 MPH]	30	30	30
Urban 70 km/h and greater [≥ 45 MPH]	100	100	100
Rural	150	150	150
Expressway/Freeway	300	450	800

NOTE:

1. Signs are shown for one direction of travel only. Place devices similar to those depicted for the opposite direction of travel.
2. Final location and spacing of signs and devices may be changed to fit field conditions as approved by the CO.
3. For pilot car operation, mount the PILOT CAR FOLLOW ME (G20-4) sign at a conspicuous location on the rear of vehicle. Prominently display the name of the contractor on the pilot car.
4. If closure is completely within the project limits, eliminate the "ROAD WORK AHEAD" (W20-1) and "END ROAD WORK" (G20-2) signs.
5. For night time flagging operation, provide floodlighting at flagger stations.
6. For project specific minimum width, refer to Special Contract Requirements, Section 156.
7. Do not allow equipment, materials, or vehicles to be parked or stored in the buffer space.
8. If signs will be in place more than 72 consecutive hours, use ground-mounted post.



NO SCALE

U.S. DEPARTMENT OF TRANSPORTATION
 FEDERAL HIGHWAY ADMINISTRATION
 FEDERAL LANDS HIGHWAY
 METRIC STANDARD
**TEMPORARY TRAFFIC CONTROL
 SINGLE LANE CLOSURE LAYOUT
 (WITH FLAGGERS)**
 STANDARD APPROVED FOR USE 6/2005
 REVISIONS:
 DRAFT: 1/2007
 STANDARD M635-6